

OUT16/19991

Mr Ian Sheppard
Manager, Environment and Community
Tahmoor Colliery
PO Box 100
TAHMOOR NSW 2573

Dear Mr Sheppard,

**Approval Conditions 14.1 and 14.2 of Tahmoor Colliery
Longwalls 27 to 30 SMP for Longwall 30 extraction to 1000 metres**

I refer to the following management plans submitted by the leaseholder:

- a) Tahmoor Colliery – Longwalls 29 to 30 Management Plan for Longwall Mining beneath the Main Southern Railway, Rev B, signed by the representatives of Australian Rail Track Corporation (ARTC) and the Leaseholder on 12 June 2015;
- b) Letter by Manager Environment and Community, Tahmoor Colliery, dated 14 March 2016 and titled "*Request LW30-01 - Request for confirm continuation of Railway Subsidence Management Plan for Longwall 30*";
- c) Tahmoor Underground Glencore Governance Arrangements Longwall Mining beneath the Main Southern Railway Revision F1 (Longwall 29 & 30), signed by the Leaseholder on 25 November 2015 and received by the Department by e-mail on 25 November 2015 at 15:13;
- d) Glencore: Tahmoor Colliery – Longwalls 28 to 30 Management Plan for Potential Impacts to Built Structures, Rev A, signed by the Leaseholder on 8 April 2014;
- e) Glencore: Tahmoor Colliery – Longwalls 28 to 30 Management Plan for Potential Impacts to Sydney Water Potable Water Infrastructure, Rev A, signed by the representatives of Sydney Water and the Leaseholder on 13 February 2014 and 31 March 2014, respectively;
- f) Glencore: Tahmoor Colliery – Longwalls 28 to 30 Management Plan for Potential Impacts to Sydney Water Sewer Infrastructure, Rev A, signed by the representatives of Sydney Water and the Leaseholder on 13 February 2014 and 3 April 2014, respectively;
- g) Management Plan Longwall Mining (LW 29 & LW30) beneath Telstra Plant @ Tahmoor & Picton N.S.W., signed by the representatives of Telstra and the Leaseholder on 7 May 2015 and 8 May 2015, respectively;

- h) Glencore: Tahmoor Colliery – Longwall 30 Management Plan for Potential Impacts to Jemena Gas Infrastructure, Rev A, signed by the representatives of Jemena and the Leaseholder on 5 May 2016 and 6 May 2016, respectively;
- i) Xstrata Coal: Tahmoor Colliery – Longwalls 27 to 30 Management Plan for Potential Impacts to Endeavour Energy Infrastructure, Rev B, signed by the representatives of Endeavour Energy and the Leaseholder on 16 October 2012 and 19 October 2012, respectively, and
- j) Glencore: Tahmoor Colliery – Longwalls 28 to 30 Management Plan for Potential Impacts to Wollondilly Shire Council Infrastructure, Rev B, signed by the representatives of Wollondilly Shire Council and the Leaseholder on 9 April 2014.

Pursuant to Conditions 14.1 and 14.2 of Executive Director Mineral Resources' Approval of SMP (Subsidence Management Plan) for Longwalls 27 to 30 (File No. 11/3219, dated 31 October 2012), I hereby approve the aforementioned management plans subject to the following conditions:

1. Definitions

1.1 ARTC - Australian Rail Track Corporation Ltd.

1.2 ONRSR – NSW Branch of the Office of National Rail Safety Regulator.

1.3 Main Southern Railway - The railway leased and managed by ARTC that runs from Macarthur in the south of Sydney to Albury near the NSW/Victorian border, including all rail infrastructure assets associated with the railway and situated within the railway corridor. In this Approval, the Term "Main Southern Railway" refers to any sections of the railway as defined above, which may be affected by subsidence as a result of extraction of Longwall 30.

1.4 Railway operations – This term means any of the following:

- a) The construction of a railway, railway tracks and associated track structures or rolling stock;
- b) The management, commissioning, maintenance, repair, modification, installation, operation or decommissioning of rail infrastructure;
- c) The commissioning, maintenance, repair, modification or decommissioning of rolling stock;
- d) The operation or movement, or causing the operation or movement by any means, of rolling stock on a railway (including for the purposes of construction or restoration of rail infrastructure), and
- e) The movement, or causing the movement, of rolling stock for the purposes of operating a railway service.

1.5 Railway Management Plan – The risk management plans as listed under Conditions 2.1 to 2.3.

1.6 SMP Approval - Executive Director Mineral Resources' Approval of SMP (Subsidence Management Plan) for Longwalls 27 to 30 (File No: 11/3219, dated 31 October 2012).

Note: for the other terms used in this Approval, refer to Schedule 2 (Definitions) of the SMP Approval.

2. All risk management measures and procedures as detailed in the management plans listed below must be implemented and fully complied with by the Leaseholder:
 - 2.1 Tahmoor Colliery – Longwalls 29 to 30 Management Plan for Longwall Mining beneath the Main Southern Railway, Rev B, signed by the representatives of Australian Rail Track Corporation (ARTC) and the Leaseholder on 12 June 2015;
 - 2.2 Letter by Manager Environment and Community, Tahmoor Colliery, dated 14 March 2016 and titled "*Request LW30-01 - Request for confirm continuation of Railway Subsidence Management Plan for Longwall 30*";
 - 2.3 Tahmoor Underground Glencore Governance Arrangements Longwall Mining beneath the Main Southern Railway Revision F1 (Longwall 29 & 30), signed by the Leaseholder on 25 November 2015 and received by the Department by e-mail on 25 November 2015 at 15:13;
 - 2.4 Glencore: Tahmoor Colliery – Longwalls 28 to 30 Management Plan for Potential Impacts to Built Structures, Rev A, signed by the Leaseholder on 8 April 2014;
 - 2.5 Glencore: Tahmoor Colliery – Longwalls 28 to 30 Management Plan for Potential Impacts to Sydney Water Potable Water Infrastructure, Rev A, signed by the representatives of Sydney Water and the Leaseholder on 13 February 2014 and 31 March 2014, respectively;
 - 2.6 Glencore: Tahmoor Colliery – Longwalls 28 to 30 Management Plan for Potential Impacts to Sydney Water Sewer Infrastructure, Rev A, signed by the representatives of Sydney Water and the Leaseholder on 13 February 2014 and 3 April 2014, respectively;
 - 2.7 Management Plan Longwall Mining (LW 29 & LW30) beneath Telstra Plant @ Tahmoor & Picton N.S.W., signed by the representatives of Telstra and the Leaseholder on 7 May 2015 and 8 May 2015, respectively;
 - 2.8 Glencore: Tahmoor Colliery – Longwall 30 Management Plan for Potential Impacts to Jemena Gas Infrastructure, Rev A, signed by the representatives of Jemena and the Leaseholder on 5 May 2016 and 6 May 2016, respectively;
 - 2.9 Xstrata Coal: Tahmoor Colliery – Longwalls 27 to 30 Management Plan for Potential Impacts to Endeavour Energy Infrastructure, Rev B, signed by the representatives of Endeavour Energy and the Leaseholder on 16 October 2012 and 19 October 2012, respectively, and
 - 2.10 Glencore: Tahmoor Colliery – Longwalls 28 to 30 Management Plan for Potential Impacts to Wollondilly Shire Council Infrastructure, Rev B, signed by the representatives of Wollondilly Shire Council and the Leaseholder on 9 April 2014.

3. Limits on Approval

- 3.1 This approval will cease to have force and effect when the length of Longwall 30 extraction reaches 1000m, or
- 3.2 Subsidence arising from the extraction of Longwall 30 must not cause track closure for any period that is unacceptable to ARTC, or
- 3.3 Subsidence arising from the extraction of Longwall 30 must not impact on the safety of railway operations on the Main Southern Railway.

4. The Leaseholder must immediately notify:

- i. ONRSR;
- ii. ARTC, and
- iii. Principal Subsidence Engineer,

of the following:

- a) Any significant deviations from the predicted magnitude of subsidence parameters and the consequential effects of such deviations on the Main Southern Railway;
- b) If continued longwall mining is forecast to exceed the limits specified in Conditions 3.2 and 3.3;
- c) If there is a malfunction or failure of the risk controls and/or monitoring systems that have been implemented in accordance with the Railway Management Plan;
- d) Any cessation of longwall operation, and
- e) If the ARTC or ONRSR advises that cessation of longwall mining is necessary.

Note: *The longwall cessation required under Condition 4d) does not refer to longwall stops for routine maintenance purposes or other stops of similarly short duration.*

5. The Leaseholder must submit a Subsidence Management Status Report specifically prepared for the Main Southern Railway, which:

- 5.1 Addresses the requirements in Condition 17(a) to 17(g) (of the SMP Approval) specifically in relation to the Main Southern Railway, and
- 5.2 Forecasts whether or not continued longwall mining is likely to exceed the limits specified in Conditions 3.2 and 3.3.

The Subsidence Management Status Report must be prepared by suitably qualified and experienced engineers and/or other specialists. It must be certified by the Leaseholder and submitted weekly to the ONRSR, ARTC and Principal Subsidence Engineer from the date when the Leaseholder receives this Approval.

The aforementioned weekly frequency to undertake the forecasts per Condition 5.2 and to submit the Subsidence Management Status Report must not be varied unless the Leaseholder complies with the three criteria stated in Section 4.3. of the Management Plan noted at Condition 2.1 above.

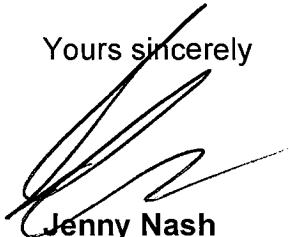
6. The ONRSR and ARTC must be notified by the Leaseholder of this approval within 24 hours of receiving this approval.

7. In the event of any inconsistency between the conditions of this Approval and the Railway Management Plan as listed under Conditions 2.1 to 2.3 above, the conditions of this Approval prevail to the extent of any inconsistency.

The Leaseholder must implement all risk controls in a timely manner to ensure, so far as is reasonably practicable, the safety of railway operations on the Main Southern Railway.

If you have any further enquires do not hesitate to contact Mr Paul Langley, Subsidence Executive Officer on 02 4931 6448.

Yours sincerely



Jenny Nash

~~A~~ Director of Mine Safety Operations

18 May 2016.